



FIRE ESCAPE ENGINEERS

A MEMBER OF THE FIRE ESCAPE SERVICES NETWORK

PRE-LOAD TEST EVALUATION RESULTS

FAILED MINOR

Imminent Safety Hazard



PERFORMED AT: **E**
side
1-5 Winchester St Brookline MA

AUTHORITY HAVING JURISDICTION:
EMILY NOEL

INSPECTION DATE **February 6 2025**
REPORT DATE **March 10 2025**
REPORT EXPIRES **April 10 2025**

PREPARED BY:
FIRE ESCAPE ENGINEERS

NOT to be used as a Construction Control Document unless noted and Formal Repair Report attached.

This document expires 30 days from date of delivery to owner/agent via email or mail unless Design Professional or Other are retained for Engineer Oversight in writing.



PRE-LOAD TEST INITIAL EVALUATION PASS/FAIL REPORT

E 1-5 Winchester St Brookline MA
side

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Site Inspection Date

pg 2



Overall Structural	Overall Paint	Supports/ Cement	Grating/ Platforms	Rails	Stringers	Treads	Cantilever/ Balanced	Fixed Ladder	Cement Pads & Footings	Catwalk
<input type="checkbox"/> Life Safety (LS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/> Imminent Safety Hazard (ISH)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Missing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> PreExisting NonConforming (PENC)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/> Poor/Fail	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> PASS other evidence of strength	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Not Applicable (N/A)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
25%	Percent Fail	0-25%	25%	25%	25%	N/A	N/A	N/A	N/A	N/A

2022 IFC 1104.16.5.1 Fire escape stairs must be examined every 5 years by a design professional or others acceptable to the Authority Having Jurisdiction and inspection report must be submitted to the AHJ.

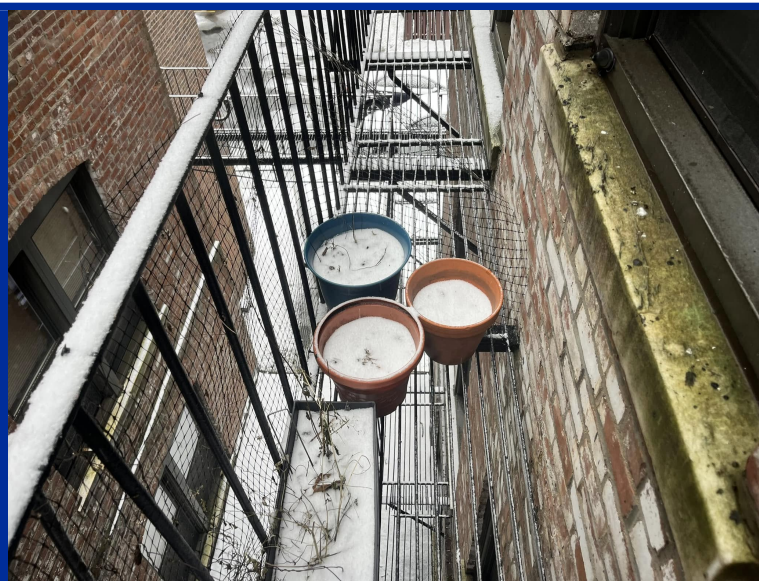
IBC 1001.3.3 All fire escapes shall be examined and/or tested and certified every five years by a design professional or others acceptable who will then submit an affidavit city official.

ICC 2015 104.7.2 Technical assistance. To determine the acceptability of technologies, processes, products, facilities, materials and uses attending the design, operation or use of a building or premises subject to inspection by the fire code official, the fire code official is authorized to require the owner or agent to provide, without charge to the jurisdiction, a technical opinion and report. The opinion and report shall be prepared by a qualified engineer, specialist, laboratory or fire safety specialty organization acceptable to the fire code official and shall analyze the fire safety properties of the design, operation or use of the building or premises and the facilities and appurtenances situated thereon, to recommend necessary changes. The fire code official is authorized to require design submittals to be prepared by, and bear the stamp of, a registered design professional

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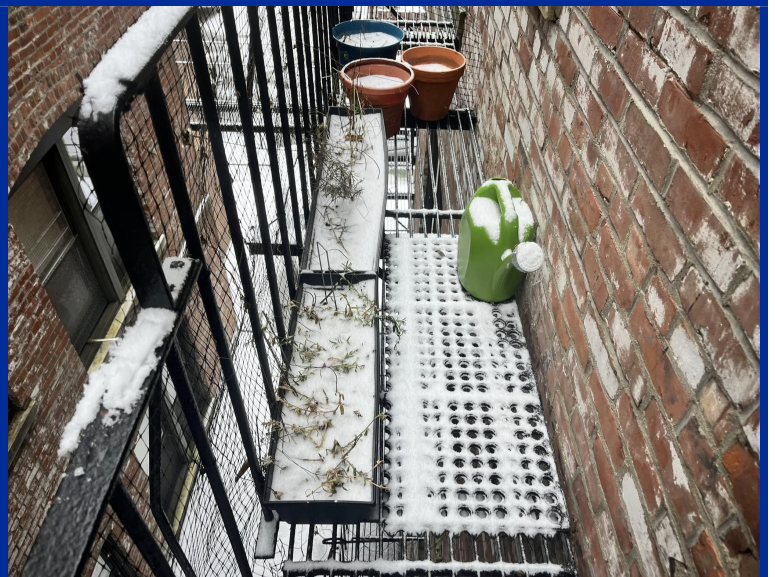




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E
side

1-5 Winchester St Brookline MA

Paint Requirements - Surface rust, sealant, and greasing:

1) The fire escape system is made of:

- IRON/STEEL
 METAL (OTHER)
 WOOD
 MASONRY
 GLASS
 MIXED
 H. DEPOT LADDER
 PAINTED
 STAINED
 MEMB
 ALUMINUM
 STEEL & WOOD
 WOOD/COMPOSITE
 CONCRETE
 BRICK
 TILED
 NO FE ON THE BLDG
 GALVANIZED
 PRIMED
 PRESSURE TREATED
 OTHER

2) This Fire Escape System is maintained/painted/stained and/or weatherproofed.

Pass

Overall Paint PASS. Recommend to power wash and seal all major joints to prevent water intrusion into structural connections.

3) The owner is notified, by email or hand delivered, that EPA Lead Paint Rules apply because the FE system was built before 1978.

Fail

Overall Paint FAIL: EPA rules apply for Lead Paint 1978. Renovator's license required. Welding not approved as repair method.

Structural Requirements - internal rust, rebolting, reinforcement and replacement:

4) All welds PASS by visual observation only, unless noted: re-bolted, x ray or load tested are structurally sound having NO internal rust jacking, external surface rust and/or material deterioration.

N/A

Not Applicable

5) Overall - fire escape system

is structurally sound having NO internal rust jacking, external surface rust and/or material deterioration.

ISH

IMMINENT SAFETY HAZARD: Overall Major Structural FAIL. Material loss and/or damaged component must be reinforced or replaced new.

6) Footings/Piers

are structurally sound having NO internal rust jacking, external surface rust and/or material deterioration - NO heaving or sinking

N/A

Not Applicable

7) Walls of attached fire escape system - by visual observation only on date of evaluation appear to be structurally sound having NO material deterioration - NO structural cracks/deterioration, deflection or bulging

Pass RLT

Overall Structural Walls PASS.

8) Supports into masonry wall

are structurally sound having NO material deterioration - NO structural cracks/deterioration, rust jacking, deflection or spalling

Fail Minor

Overall Minor Structural Supports (bracket, thru-bolt, legs) FAIL: Cracking around some connections

9) Thru bolts into wood structure or masonry walls

are structurally sound having NO material deterioration - NO structural cracks/deterioration, rust jacking, deflection or wood rot

N/A

Not Applicable

10) Platforms, Slats, Grating, Mesh, Cement, Cast Iron, Wood and Angle and or Steel

Frame are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust or wood rot

ISH

IMMINENT SAFETY HAZARD: Overall Major Structural Platforms, Slats, Grating FAIL. Material loss and/or damaged component must be reinforced or

11) Stair Stringers, Upper & Lower Hanger Clips

are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust or wood rot

N/A

Not Applicable

12) Stair Treads: Plate, Slats, Grating & Bolts and/or Welds

are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust or wood rot

N/A

Not Applicable

13) Railings - on platforms, stairs & catwalks

are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust or wood rot

Fail Minor

Overall Minor Structural FAIL. Internal rust causing Rust Jacking (RJ) of some suspect connections.

14) Fixed Ladders to Roof and/or to Grade & Bolts and/or Welds

are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust or wood rot or spalling

N/A

Not Applicable

15) Balanced Ladders: Bolts and/or Welds, Weight, Release Mechanism and other components

are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust and are to grade/public way

N/A

Not Applicable

16) Cantilevers: Bolts and/or Welds, Weight Box, Release Mechanism and other components

are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust and are to grade/public way

N/A

Not Applicable

17) Catwalks & Bolts and/or Welds are structurally sound having NO material deterioration - NO internal rust jacking or external surface rust and lead to the fire escape and completes to grade

N/A

Not Applicable

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Code Requirements - Fabrication, Installation, Modification, and Code Upgrades

18) All fabrication, installation and maintenance of fire escape is to code and met industry standards on date of installation.

Pass

Design and Fabrication met code standards on date of installation.

19) There are **NO** pre-existing non-conforming issues requiring AHJ notification for approval.

Pass

NO AHJ approval required.

20) All components: doors, windows, window guards, cages and gates are single action requiring no special knowledge, no keys and no obstruction and lead to public way

Pass

Design and Fabrication met code standards on date of installation.

21) All electrical power is 10 feet or more away from fire escape or covered to code.

Pass

Design and Fabrication met code standards on date of installation.

22) Overall the fire escape is not illuminated due to pre-existing code on date of install.

Code

Overall Code Issues: NO illumination visible. Local Ordinance may apply to upgrade egress lighting.

23) Overall fire escape system has no interior or exterior obstructions such as a/c units, plants, bikes, trash etc.

Fail

Overall Code Issues: Obstruction, Debris/Trash/Plant Pots/Barriers, need to be removed.

24) Overall fire escape system has no storage of flammables or code restricted items on, in or

Pass

Overall Code Issues: NO Flammables on or under system i.e. BBQ or anything powered by gasoline.

25) Do all egress systems allow for clear and legal access to public fairway or dispersal area?

Pass

Overall Code Issues: NOT Blocked and has access to public way.

ALL FIRE ESCAPES MUST BE STRUCTURALLY SOUND AND KEPT PAINTED AS PER CODE. Structural connections must be free of all internal rust and sealed from water intrusion. Spot paint every 3-5 years, full paint every 7-10 years and maintain sealant on all critical structural connections.

IFC 1104.16.5.1 Fire escape stairs must be examined every 5 years by a design professional or others acceptable to the Authority Having Jurisdiction and inspection report must be submitted to the AHJ. IBC 1001.3.3 All fire escapes shall be examined and/or tested and certified every five years by a design professional or others acceptable who will then submit an affidavit city official. NFPA LIFE SAFETY CODE 101 7.2.8.6.2 The Authority Having Jurisdiction (AHJ) shall approve any fire escape by Load Test or Certification (other evidence of strength). ICC 104.7.2 Technical assistance. The fire code official is authorized to require the owner or agent to provide, without charge to the jurisdiction, a technical opinion and report. The opinion and report shall be prepared by a specialist or a fire safety specialty organization acceptable to the fire code official to analyze the fire escape and appurtenances situated thereon, to recommend necessary changes. The fire code official is authorized to require design submittals to be prepared by, and bear the stamp of, a registered design professional. OSHA 1910.37 Exit routes must be maintained during construction, repairs, alterations or provide alternative egress with equivalent level of safety. (permit issued if egress is certified or with egress scaffolding) All insurance companies: All final certifications to AHJ (load tested or other evidence of strength) must be submitted for acceptance by insurance company to avoid coverage issues.

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E 1-5 Winchester St Brookline MA
side

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1. Overall Structural:

Poor/Fail

25%

This pre load test evaluation was requested at this location to confirm that the fire escape system is structurally sound and has been kept painted as is required by code. Certification can be done by load test, other evidence of strength or an opinion affidavit with a disclaimer of liability waiver form. This is a four story brick building with a fire escape system made of painted steel and consisting of crossovers. This system did not pass due to the following minor issues and imminent safety hazard:

SUPPORTS: Minor cracking has been observed around embedded structural members, which could lead to water infiltration and potential deterioration over time. These cracks should be properly sealed and repaired to prevent further damage and maintain the integrity of the system.

Grating/ Platforms: Imminent Safety Hazard - The platform spacers are deteriorating, causing instability, particularly in one section where the slats have become loose. This poses a safety hazard and requires immediate reinforcement or replacement to restore structural integrity and ensure secure footing.

Rails: There is a broken connection on the first floor, which compromises the structural integrity and safety of the system.

Other:

- There are obstructions on the platform, which could hinder safe egress and create a tripping hazard. These obstructions should be removed or relocated to ensure a clear, unobstructed path that complies with safety regulations.
- The existing lighting may require an upgrade to meet current code requirements and ensure adequate illumination of the egress path. A compliance assessment is recommended, and additional emergency lighting should be installed as needed to enhance visibility and support safe evacuation.



2. Overall Paint:

Fair

0-25%

SEE VIDEO-PHOTO EVALUATION FOR MORE DETAILED INFORMATION * THIS IS NOT A CONSTRUCTION CONTROL DOCUMENT



Code:

Issues Exist

25%

Overall the paint PASS: Spot paint on system required after repairs. Recommend to power wash and seal all major joints to prevent water intrusion into structural connections. Fire Escapes, must be maintained/ painted every 5-7 years as per manufactures recommendation.

Our inspector found code issues related to AHJ (Authority Having Jurisdiction) or PENC (pre-existing non-conforming) requirements for this Fire Escape system.

- There are obstructions on the platform.
- The existing lighting may require an upgrade to meet current code requirements.



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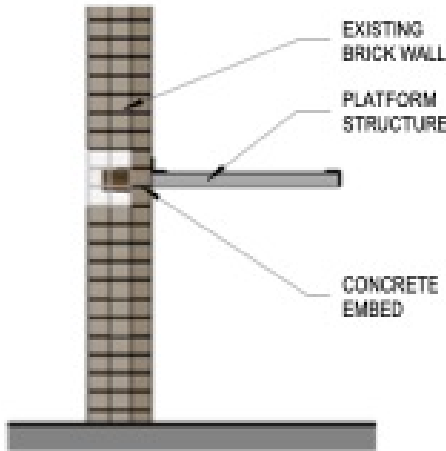
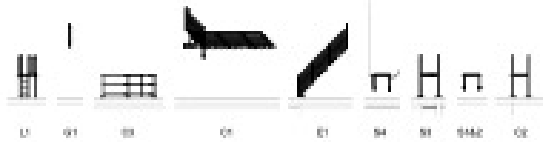
E 1-5 Winchester St Brookline MA
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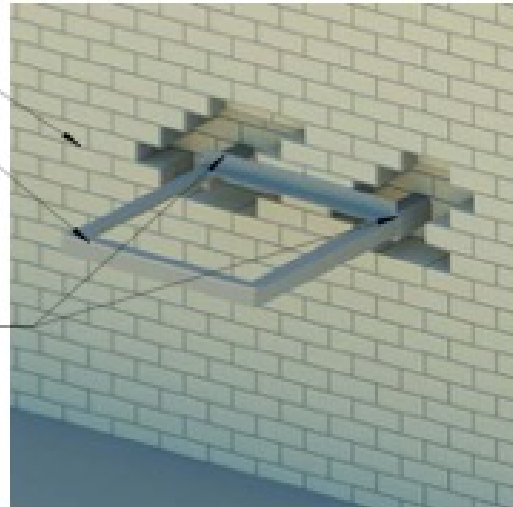
STRUCTURAL SUPPORT COMPONENTS



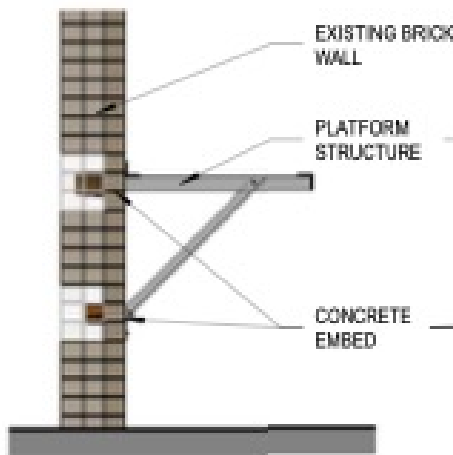
INITIAL EVALUATION
PASS/FAIL REPORT
TYPICAL HISTORICAL
EXISTING CONDITIONS



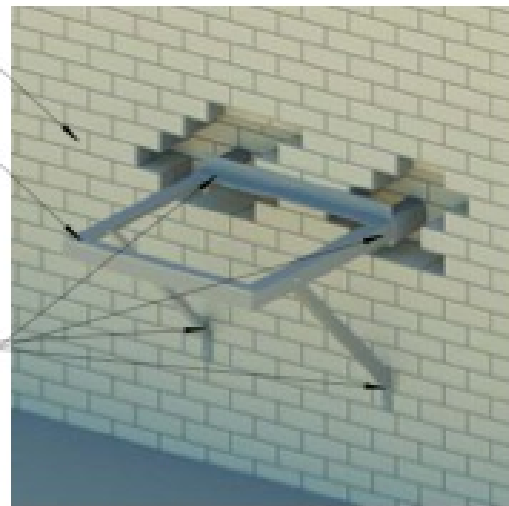
1 S1-BALC-WITH-BRACKET
SCALE: 3/8" = 1'-0"



7 S1-3D-EVAL-RENDERING-BALCONY-NO-BRACKET
SCALE: 1/2" = 1'-0"



3 S2-BALC-WITH-BRACKET
SCALE: 3/8" = 1'-0"



4 S2-3D-EVAL-RENDERING-BALC-BRACKET
SCALE: 1/2" = 1'-0"

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STRUCTURAL SUPPORT COMPONENTS

S1&2

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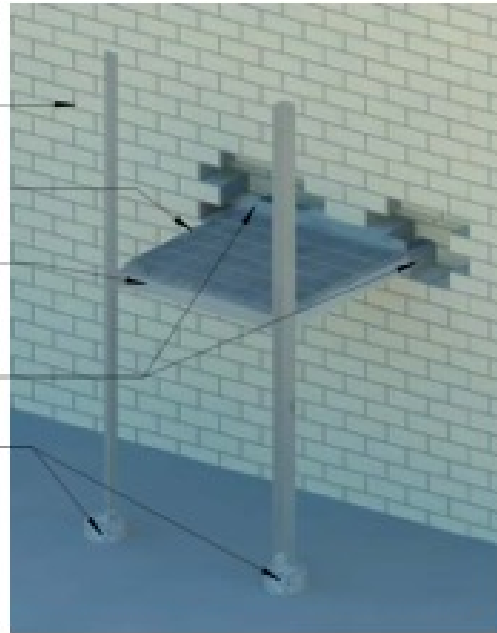
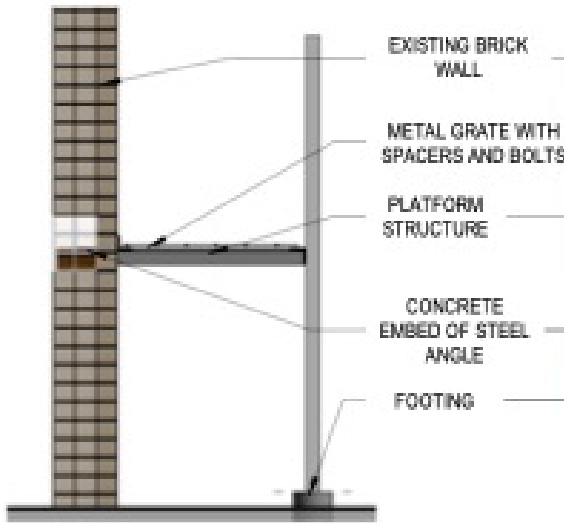
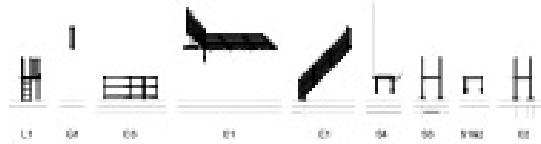
PRE-LOAD TEST INITIAL EVALUATION PASS/FAIL REPORT

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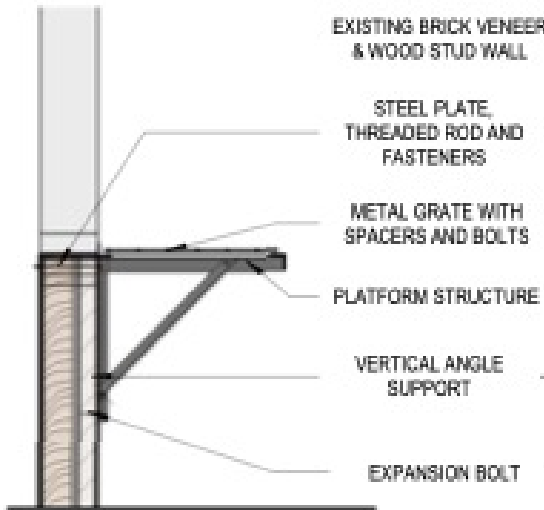


INITIAL EVALUATION
PASS/FAIL REPORT
TYPICAL HISTORICAL
EXISTING CONDITIONS



1 S3-BALC-WITH-POST-TO-GROUND
SCALE: 3/8" = 1'-0"

2 S3-3D-RENDERING-BALCONY POST TO GROUND
SCALE: 12" = 1'-0"



3 S4-BALC-WITH-THRU BOLT & PLATE
SCALE: 3/8" = 1'-0"

4 S4-3D-EVAL-RENERING-BALC-WITH THRU BOLT
SCALE: 12" = 1'-0"

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STRUCTURAL SUPPORT COMPONENTS

S3&4

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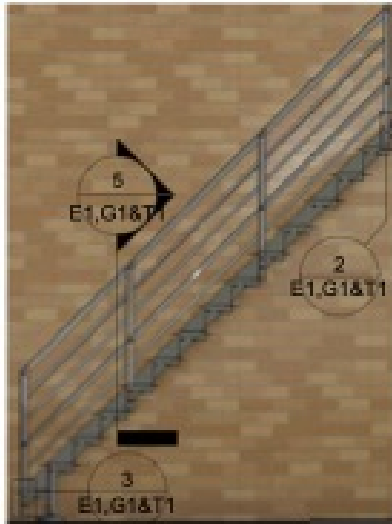
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PLATFORM and STAIRS COMPONENTS

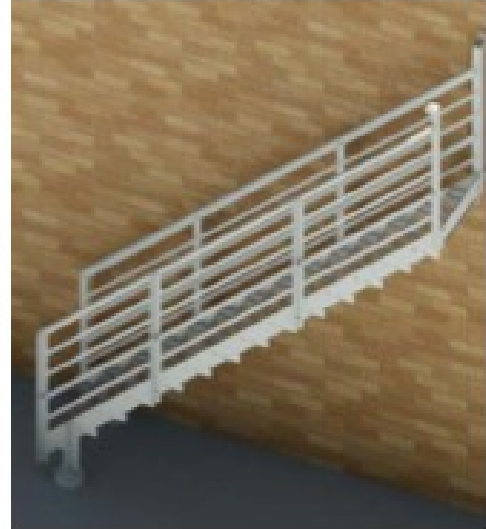


INITIAL EVALUATION
PASS/FAIL REPORT
TYPICAL HISTORICAL
EXISTING CONDITIONS



1 E1-STAIR TYPICAL
SCALE: 1/4" = 1'-0"

2 E1-TOP
SCALE: 3/4" = 1'-0"

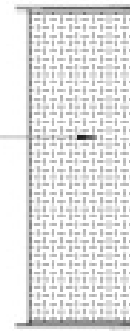


3 E1- BOTTOM **4 E1- 3D-EVAL-RENDERING-STAIR TYPICAL**
SCALE: 3/4" = 1'-0" SCALE: 1/2" = 1'-0"

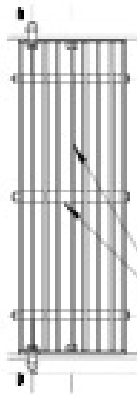


5 T1- TREAD
SCALE: 1" = 1'-0"

CHECKER PLATE TREAD MAKE 24"X24" GRATING COMPONENT. CISD WILL TELL WHETHER ITS A CONCRETE GRATE OF CHECKER PLATE

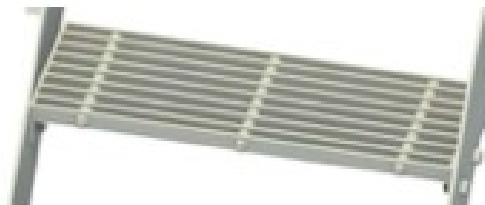


7 T2 CHECKER PLATE TREAD - PLAN
SCALE: 1" = 1'-0"



6 T1 HISTORIC TREAD - PLAN
SCALE: 1" = 1'-0"

BAR GRATE WITH SPACES AND A THRU BOLT



10 G1-3D-EVAL-TREAD
SCALE: 1/2" = 1'-0"

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TREAD & GRATING COMPONENTS

E1,G1&T1

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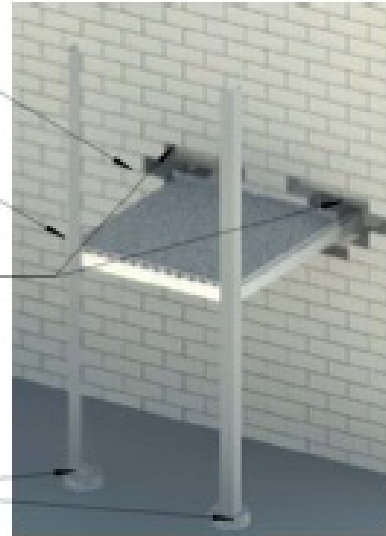
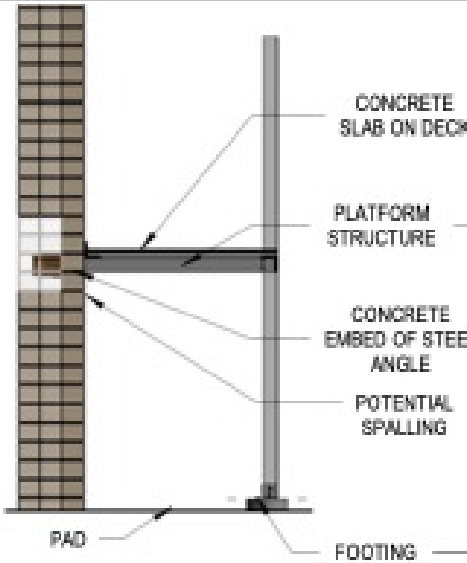
E side 1-5 Winchester St Brookline MA

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CEMENT SLAB, FOOTING & PAD COMPONENTS

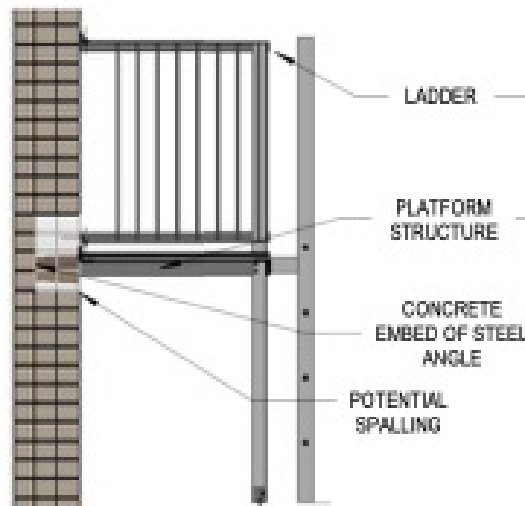


INITIAL EVALUATION
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TYPICAL HISTORICAL
EXISTING CONDITIONS



1 C2-CEMENT SLAB, FOOTING AND PAD
SCALE: 3/8" = 1'-0"

2 C2-3D-EVAL-CONCRETE FOOTING
SCALE: 12" = 1'-0"



3 L1-BALCONY AND LADDER
SCALE: 3/8" = 1'-0"

4 L1-3D-EVAL-BALCONY & LADDER
SCALE: 12" = 1'-0"

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CEMENT SLAB, FOOTING & PAD

C2 L1

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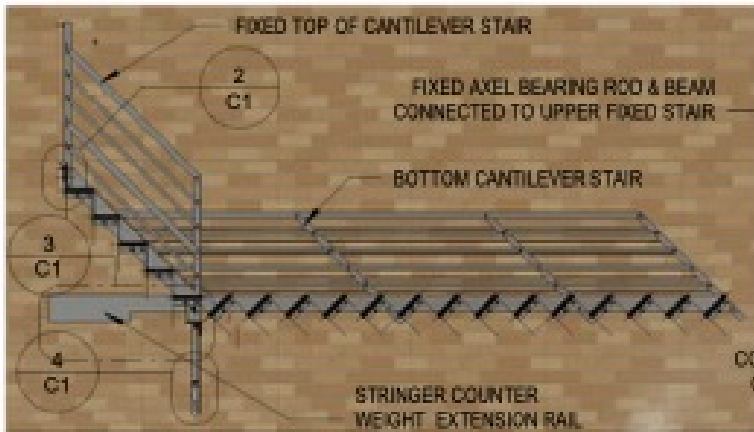
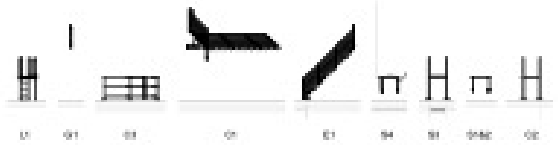
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CANTILEVER STAIRS



INITIAL EVALUATION PASS/FAIL REPORT

TYPICAL HISTORICAL
EXISTING CONDITIONS



1 C1-STAIR CANTILEVER

SCALE: 1/4" = 1'-0"

6 C1-ENLGD. 3D BEARING ROD

SCALE:



LOWER CANTILEVER
STAIR RELEASE ROD
AND PUSH BAR

2 C1-TOP

SCALE: 1/2" = 1'-0"



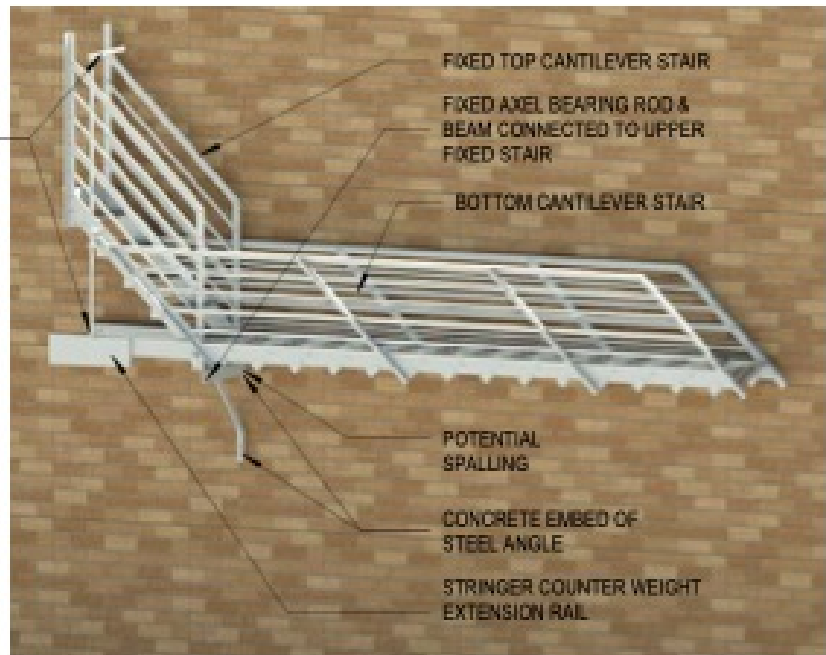
3 C1-BEARING ROD

SCALE: 1/2" = 1'-0"



4 C1-BRACKET BOTTOM

SCALE: 1/2" = 1'-0"



7 C1-3D-EVAL-RENDERING-STAIR CANTILEVER

SCALE: 1/2" = 1'-0"

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CANTILEVER STAIRS

C1

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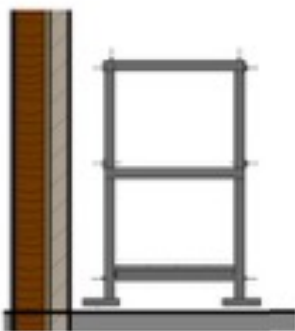
E 1-5 Winchester St Brookline MA
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CATWALK COMPONENTS



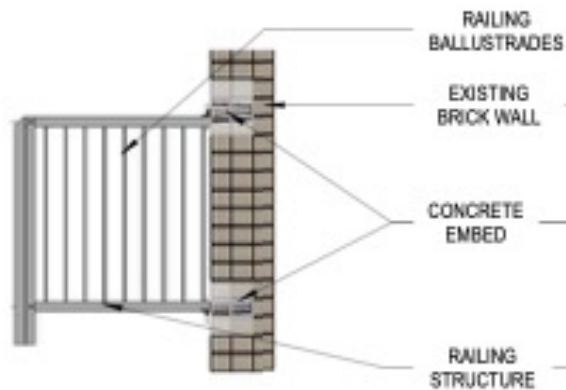
INITIAL EVALUATION
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1 C3-CATWALK AND COMPONENTS
SCALE: 3/8" = 1'-0"



2 C3-3D-EVAL-RENDERING-CATWALK
SCALE: 1/2" = 1'-0"



4 E2-RAIL WITH EMBED
SCALE: 3/8" = 1'-0"



6 E2-3D-RENDERING-RAILING
SCALE: 1/2" = 1'-0"

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CATWALK COMPONENTS

C3&E2

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E 1-5 Winchester St Brookline MA

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1-5 Winchester St
Site Address
Sal Monteneri
Owner or Owner Agent
Brookline MA 02446
City State Zip Code
(617) 728-2759
Phone fax
smonteneri@dlsaunders.com
email



FE Structural Format **DL Saunders**
Crossover Balcony Owner or Owner Agent Company
Location **E** Mailing Address
FE Made Of **Steel** City **Boston** State **MA** Zip Code
Stories **4** Phone fax
of FEs on building **5** website

Brookline MA 333 Washington St 3rd FL Brookline MA 02445
Authority Having Jurisdiction Address City State Zip Code
enoel@brooklinema.gov 617-730-2100 617-739-7542
email Phone fax
Violation Number WRITTEN VIOLATION VERBAL N

Repair/Paint Vendor or Owner/Agent acceptable by AHJ to repair/paint fire escapes:
Repair Vendor or Agent: Company Name Repair Vendor or Agent: Contact Name License Number Repair Permit Number

This document is a. a Pass/Fail Report (not to be used as a construction control document)
OR b. a Fail Report with attached repairs report (with photos/drawings and repairs criteria as required for permit if permit is needed)

To the best of my Information, Knowledge, Belief and Opinion that the following statements are true and apply regarding this Fire Escape System as of the date of evaluation above.

Indicate inspection was done by:

- a. a visual walk through of the Fire Escape System all accessible areas only. NO Load Test or destructive testing was performed. Safety hammer testing, scraping, poking, and chipping are all part of visual observation and resulting damage is owner's responsibility to repair or maintain.
- b. an Evaluation of the Fire Escape System from the ground, with or without the use of visual aid, due to NO ACCESS or UNSAFE STRUCTURE WITH LIFE SAFETY ISSUES.

Fire Escape Passed? FAILED MINOR

Life Safety Imminent Safety Hazard

- The System is Certified by Load Test done by Engineering/Testing Agency at 100 lbs per sq foot.
- The System is Certified by Other Evidence of Strength (by full restoration or NEW) in lieu of load test.
- The System is Certified by Opinion Affidavit, accepted by AHJ as ready for use with opinion disclaimer of liability.
- Inspection/Evaluation determined the System FAILED. It is NOT structurally sound and/or painted.

ALL FIRE ESCAPES MUST BE STRUCTURALLY SOUND AND KEPT PAINTED AS PER CODE. Structural connections must be free of all internal rust and sealed from water intrusion. Spot paint every 3-5 years, full paint every 7-10 years and maintain sealant on all critical structural connections.

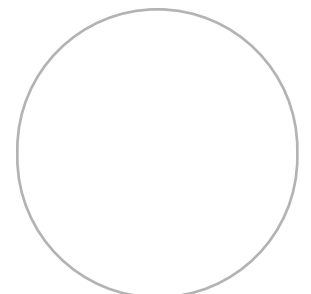
IFC 1104.16.5.1 Fire escape stairs must be examined every 5 years by a design professional or others acceptable to the Authority Having Jurisdiction and inspection report must be submitted to the AHJ. IBC 1001.3.3 All fire escapes shall be examined and/or tested and certified every five years by a design professional or others acceptable who will then submit an affidavit city official. NFPA LIFE SAFETY CODE 101 7.2.8.6.2 The Authority Having Jurisdiction (AHJ) shall approve any fire escape by Load Test or Certification (other evidence of strength).

Francisco Meneses Design Professional or Other Name CS-94862 License Number Jan/07/26 License Expires Date
866-649-0333 Phone 888-895-7507 fax Feb/06/25 Site Inspection Date
info@firescapeengineers.com email 5902 Case ID

Fire Escape Engineers Design Company or Other Company Name
616 Washington St Address
Lynn MA 01901 City State Zip Code

X

2/6/2025
peer reviewed by Fire Escape Engineer Francisco Meneses



**PRE-LOAD TEST INITIAL EVALUATION PASS/FAIL REPORT****E**
side**1-5 Winchester St Brookline MA**Feb 06 2025
Site Inspection Date**Responsible Party Assignment as per Code****Fire Escape Inspectors Oversight Options:**

Further information for each option you may have interest in will be sent with a formal proposal upon request. Please reach out after reviewing these options with any questions or concerns. A signature is required with a 35% deposit to initiate any option.

Project Management Oversight (PMO): Oversee our network Vendor, or a Vendor of your choice, to do the work necessary to bring your fire escape into compliance and is certified by a final examination (a Load Test may still be required). Technical Repair report written for project. 3 to 5 Visits with a design professional. Zoom meeting to review all repairs and methodology with the chosen vendor is included. Pictures or video supplied by the vendor on a daily basis. unlimited phone calls or Facetime. Includes final certification. Can be a standalone service if you choose your own vendor.

Vendor Management Oversight: Oversee our Network Vendor, or a Vendor of your choice (who is a fully experienced fire escape technician), to do the work necessary to bring your fire escape into compliance and is certified by a final examination (a Load Test may still be required). Zoom meeting to review all repairs and methodology with the chosen vendor is included. Pictures or video supplied by the vendor on a daily basis. Unlimited phone calls or Facetime. Includes final certification. Can be a standalone service if you choose your own vendor.

Load Test: A stand-alone Load Test can be performed with a Deficiency Report (identifying outstanding distressed conditions not repaired or repaired poorly) if you choose your own vendor for the repair process. A Load Test is performed at 100 lb. per square foot and 200 lb. Lateral force on all railings. Weight used depends on square footage calculated A 5-year certification will be issued for one inspection cycle only.

Fire Escape Services: Restoration - Repairs - Repainting 4 Option Pricing

Full Restoration and Full Paint: Restoration will begin shortly after Emergency Repairs are completed if needed. All major structural connections will be cleaned, primed, sealed, and re-bolted. Any components with more than 25% material in any area will be reinforced or replaced. All Minor connections with internal rust will also be treated in the same manner. All Surface Rust is scraped, primed, sealed with a 50-year paintable silicone, and a full topcoat is applied following EPA Lead guidelines (DTM or Oil Base). After final examination by our Inspector (approved by AHJ) a certification with a 15-25-year structural warranty will be issued. This service will negate any future Load test requirement for up to 25 years with AHJ approval. Includes PMO

Spot Restoration and Full Paint All major connections with excessive internal rust will be cleaned, primed, sealed, and re-bolted. Any minor connections with excessive internal rust will be treated the same. Any components with more than 25% material in any area will be reinforced or replaced. There will be an Integrated Load Test and Dynamic Stress Test to components that were not restored. All Surface Rust is scraped, primed, sealed with a 50-year paintable silicone, and a full topcoat is applied following EPA Lead guidelines (DTM or Oil Base). After final examination by our Inspector (approved by AHJ) a certification with a 10-year structural warranty will be issued on only work performed. The cost of the Spot Restoration can be credited towards a Full Restoration within 5 years (price adjusted for inflation and labor rates at that time), some restrictions may apply. Includes Integrated Load Test and Dynamic Stress Test to components. Includes PMO.

Spot Repair and Spot Paint: Only distressed, extremely poor welding, and/or broken connections will be repaired as needed. Critical deteriorated material will be reinforced or replaced as needed. The entire system will be certified (for 5-years only) by a Full Load Test and Dynamic Stress Test. A Spot Paint includes scraping and priming: loose or peeling paint, surface rust, and repaired areas. A spot topcoat is applied following EPA Lead guidelines. No warranty is offered. Includes Load Test and Dynamic Stress Test to components. Includes VO.

PRE-LOAD TEST INITIAL EVALUATION PASS/FAIL REPORT

E 1-5 Winchester St Brookline MA
side

Feb 06 2025
Site Inspection Date

